

HERITAGE IMPACT STATEMENT



24 Railway Street, Hurlstone Park

August 2022 | J5833

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1.0 INTRODUCTION

1.1 Preamble

This Heritage Impact Statement (HIS) has been prepared in conjunction with a Development Application for the removal of all structures on the site and construction of a new single storey dwelling at No.24 Railway Street, Hurlstone Park, New South Wales.

The site is located within Canterbury Council. The principal planning control for the site is the *Canterbury Local Environmental Plan 2012 (LEP 2012)*. The site is not listed as a heritage item by Schedule 5 Part 1 of the *LEP 2012* but is located within the Railway Street Heritage Conservation Area as listed by Schedule 5 Part 2 of this Plan and is also in the vicinity of heritage items listed by Schedule 5 Part 1 of the *LEP 2012*.

This statement has been prepared at the request of the owners of the property and accompanies plans prepared by Fairmont Homes NSW.

1.2 Authorship

This HIS was prepared by Suzanne Fuchs, B. Arts (Arch), B.Arch., M. Cult.Heritage, and James Phillips, B.Sc. (Arch.), B.Arch., M.Herit.Cons.(Hons.), of Weir Phillips Heritage and Planning.

1.3 Limitations

The history contained in this statement was compiled from readily available sources listed under Section 1.6 below. A search of Council records was not provided for.

An Aboriginal history and assessment were not provided for. No historical archaeology was carried out on the site.

1.4 Methodology

This HIS has been prepared with reference to the Heritage NSW publication *Statements of Heritage Impact* (2002 update) and with reference to the Council planning documents listed under Section 1.6 below.

A site visit was carried out in August 2022. Unless otherwise stated, the photographs contained in this HIS were taken on this occasion.

1.5 References

1.5.1 General References

- John Sands' Ltd, *John Sands' Sydney and Suburban Directories*, NSW, John Sands' Ltd, various dates.
- Paul Davies Pty Ltd, *Hurlstone Park Heritage Assessment Study: Stage 1 Report*: September 2016. Canterbury Council website.
- Paul Davies Pty Ltd, *Hurlstone Park Heritage Assessment Study*, Stage 2: Heritage Assessment of Potential Heritage Items, April 2017. Canterbury Council website.

1.5.2 Historic Plans and Photographs

- (Aerial photograph over Hurlstone Park), 1943. maps.six.nsw.gov.au.
- *Jeffreys Estate, Fernhill, Marrickville & Belmore Railway Line*: 1st subdivision, 1903. National Library of Australia.
- *Plan of the Canterbury estate referred to.* (1842). State Library of NSW., accessed August 2022.
- Subdivision of Part of the Canterbury Estate, 186-? National Library of Australia.

1.5.3 Heritage Listing Sheets

- *Railway Street Heritage Conservation Area*. State Heritage Inventory Database No.:5067879.

1.5.4 Planning Documents

- *Canterbury Development Control Plan 2012*.
- *Canterbury Local Environmental Plan 2012*.

1.6 Site Location

No. 24 Railway Street is located on the eastern side of Railway Street (Figure 1). The site is identified as Lot 10, Section 5, D.P. 3849.

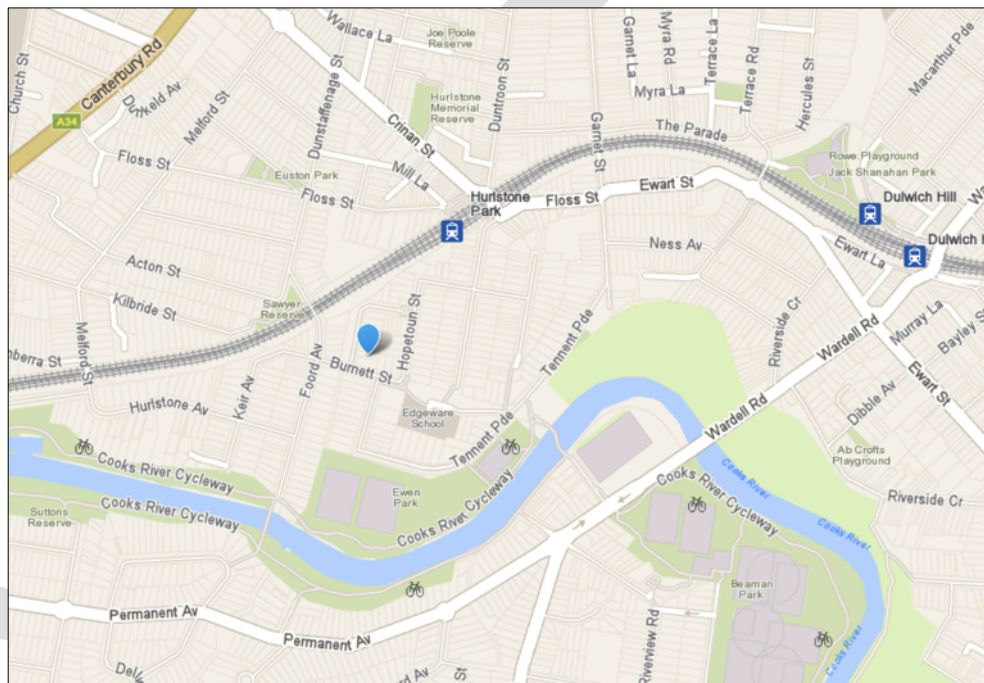


Figure 1: Site location with No. 24 Railway Street marked by the blue dot.
Whereis.com.au, accessed August 2022.

2.0 HISTORICAL DEVELOPMENT

2.1 Aboriginal History

While an Aboriginal history has not been provided for, it is acknowledged that the subject site and the immediately surrounding area lie within the traditional lands of the Wanggal people.

2.2 Early European Development

Unless otherwise specified, the information about the general history of the surrounding area is taken from Paul Davies Pty Ltd, *Stage 1 Report: Hurlstone Park Heritage Assessment Study*, September 2016. This report should be referred to for greater detail.

The British Colony of New South Wales was formally established in January 1788. The first township, Sydney, was established on the southern shores of Sydney Harbour. The colonists explored the Cook's River and its hinterland soon after settlement. Ignoring the presence of the Aboriginal people, all land was declared to be Crown land. Land was granted by successive governors to further the purposes of settlement. The first land grant in the present-day City of Canterbury was made to the Reverend Richard Johnson in 1793. Johnson called his grant 'Canterbury Vale.'

The subject site stands on part of a grant of 700 acres made to Thomas Moore on 1 October 1803. Much of present-day Hurlstone Park, including Moore's grant, was consolidated into a large estate known as the 'Canterbury Estate' by the highly successful Sydney merchant Robert Campbell (1769-1846). By the mid 1830s, Campbell owned all the land between the Cooks River at Canterbury and Liverpool Road, Ashfield, an area of around 1,242 acres. While not residing on his estate, Campbell did improve it. Access into the area during the early nineteenth century was by a track, likely to have first been established by the Aboriginal people, that later became known as Georges River Road.

The first subdivisions on the 'Canterbury Estate' occurred in the mid 1830s on the western side of the estate. Villages were established on the estate at Canterbury and South Ashfield in the 1840s. Access into the area was improved when Canterbury Road was constructed to connect the Parramatta Road with the bridge crossing of Cooks River established by Cornelius Prout in 1840. New Canterbury Road was constructed between 1849 and 1850.

Following Campbell's death in 1846, the Canterbury Estate was divided between his daughter, Sophia Ives Campbell (1812-1891), and his son-in-law, Arthur Jeffreys (1811-1861), husband of his other daughter Sarah Campbell (1815-1856). Sophia inherited 673 acres, inclusive of present-day Hurlstone Park. While Sophia never occupied her land, she did arrange for the building of St. Paul's Church and schoolhouse.

Figure 2 illustrates the extent of Campbell's Estate prior to its subdivision.



Figure 2: c.1842 Plan showing the extent of the Canterbury Estate, marked by the red circle. State Library of NSW., accessed August 2022.

2.3 The Railway Street Heritage Conservation Area

The NSW State Heritage Inventory sheet for the Railway Street Heritage Conservation Area, in which the subject site is located, provides the following history for the area:

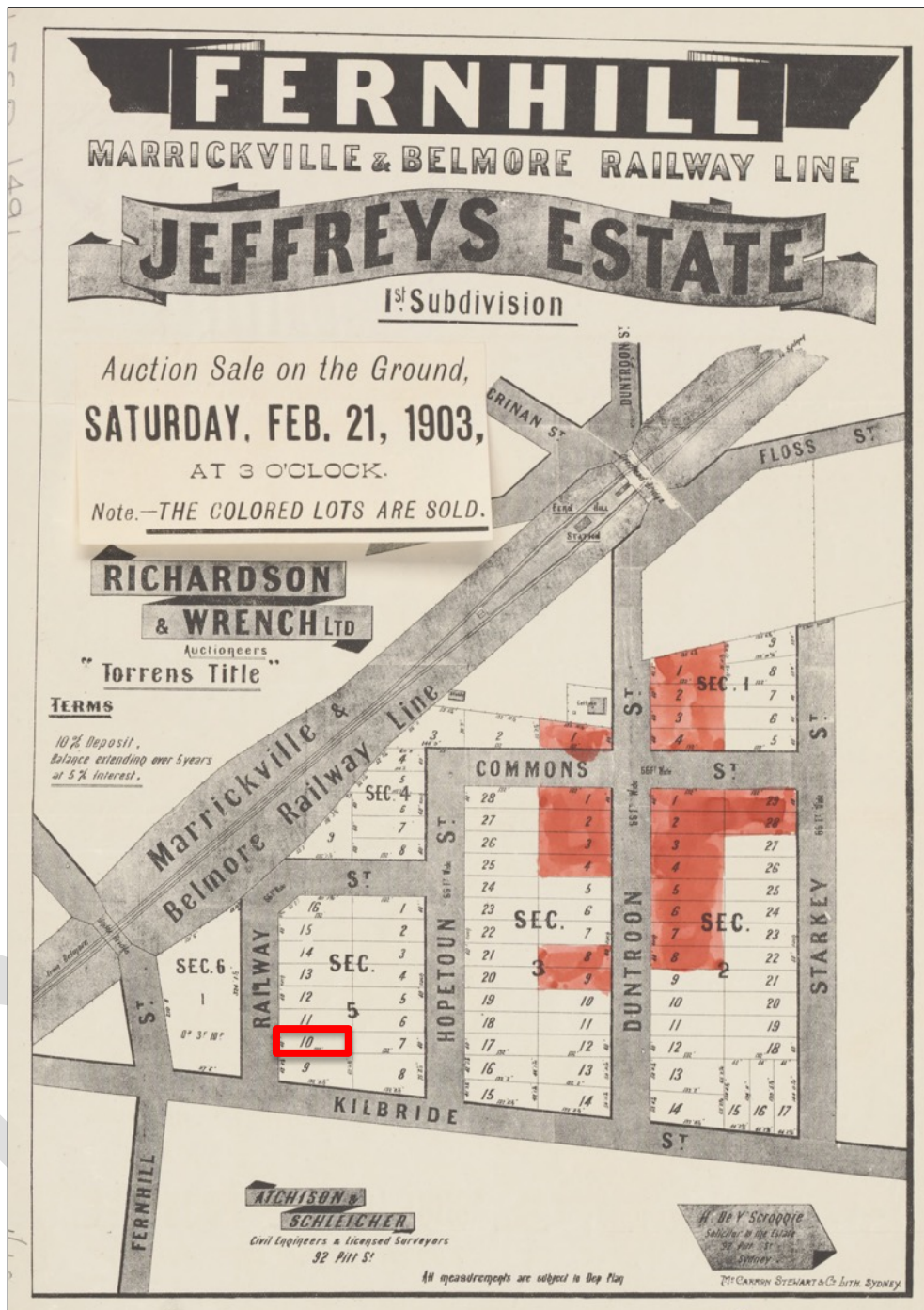
'While Hurlstone Park's first building boom took place during of the Federation era between 1901 and 1915, some development occurred within the vicinity of the railway subsequent to the opening of Fernhill (now Hurlstone Park) railway station in 1895; reflected in the houses located within the immediate vicinity of the railway corridor along Railway Street, which predate the Federation period.

This Conservation Area is comprised of land which was originally part of the 1st and 8th subdivisions of the Jeffrey's Estate. Between 1901 and 1918 there were 10 land releases at Hurlstone Park made by the Jeffreys. These land releases re-subdivided the 1870s large blocks (in DP 191) to form narrow fronted suburban allotments. The first land release made in 1901 (in DP 3849) comprised land south of the railway line between Burnett and Commons streets, and the railway line and Starkey Street. In the 1870s land release (DP 191) this area comprised six blocks, and in the re-subdivision, there were 92 house blocks. The eighth land release (DP 6787) of 1916 encompassed land south of the railway, including the properties west of Foord Avenue and north of Burnett Street. In this subdivision were formed Keir and Hurlstone Avenues and 54 building blocks. A Broadhurst postcard dated to 1912 depicts a row of substantial brick Federation houses along Hopetoun Street at this time.¹

2.4 No. 24 Railway Street

Figure 3 illustrates a real-estate advertisement for lots within the first subdivision of *Jeffreys Estate*. The subject site is Lot 10 in Section 5 of this plan. The subject site retains the same lot and section number today.

¹ *Railway Street Heritage Conservation Area*. State Heritage Inventory Database No.:5067879.



Street numbers for Railway Street are first used in the *Sands' Directories* in 1929. At this time, the occupant for No. 24 Railway Street is listed as Henry Griffiths, thus, a construction date for the original dwelling on the site of c.1908 is likely.

Henry Griffiths is listed as residing at No. 24 Railway Street until 1932/33, the last year the *Sands' Directories* was published. It is noted that the *Sands'* never lists an occupant for No. 22 Railway Street, supporting the theory that No. 22 and No. 24 Railway Street were owned by the same person until c.1970.

No information about Henry Griffiths has been found during the research for this report.

No historic photographs of the subject site at street level have been located. Figure 4 and Figure 5 provides an aerial photograph over the site and the surrounding area from 1943. The photographs illustrate that the surrounding area had been developed by this time, however, the numerous mature street trees opposite the site, and in adjoining streets are not yet visible. Figure 5 reveals that Lot 10 (No. 24), and Lot 11 (No. 22) were most likely owned by the same person as there is a single dwelling adjacent to the southern side boundary of the combined site, and gardens where No. 22 Railway Street stands today. The outline and location of the dwelling on the site at that time is similar to that which is evident today. It is noted that the garage, laundry, and brick structure toward the rear had not been constructed by 1943.



Figure 4: Aerial Photograph over the Site and the Surrounding Area, 1943. The subject site is outlined in red and marked by the red arrow.
SIX Maps accessed August 2022.

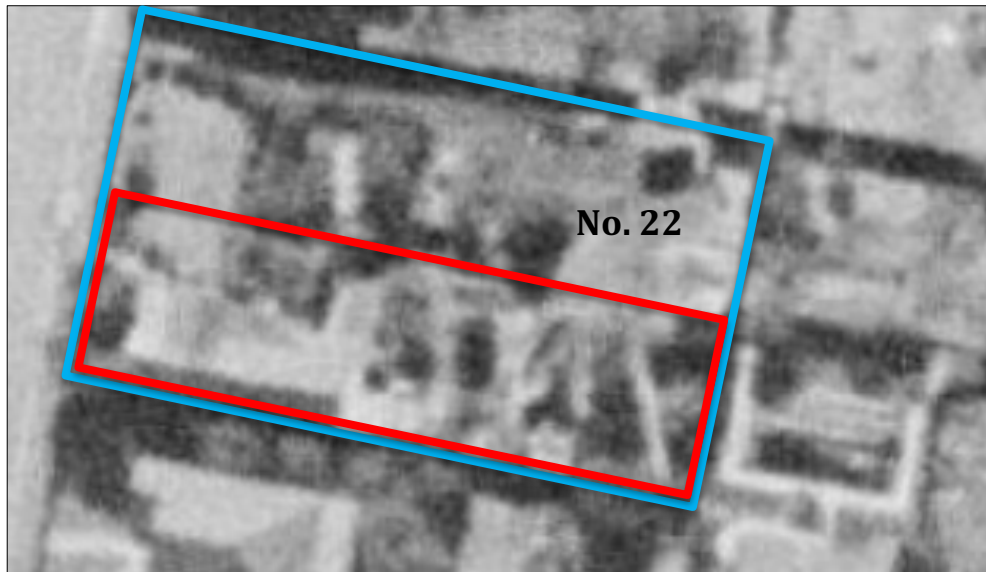


Figure 5: Detail over the subject site, outlined in red, from the 1943 aerial photograph above. The apparent boundaries of the site in 1943, comprising No. 22 and No. 24 Railway Street, are outlined in blue. SIX Maps accessed August 2022.

Figure 6 to Figure 7 illustrate the site in 1955 and 1971. These images reveal that the laundry and rear outhouse was constructed between 1943 and 1955. By 1971 the garage had been constructed on the subject site, and a separate dwelling had been constructed at No. 22 Railway Street. This is consistent with the style of dwelling visible at No.22 Railway Street today.

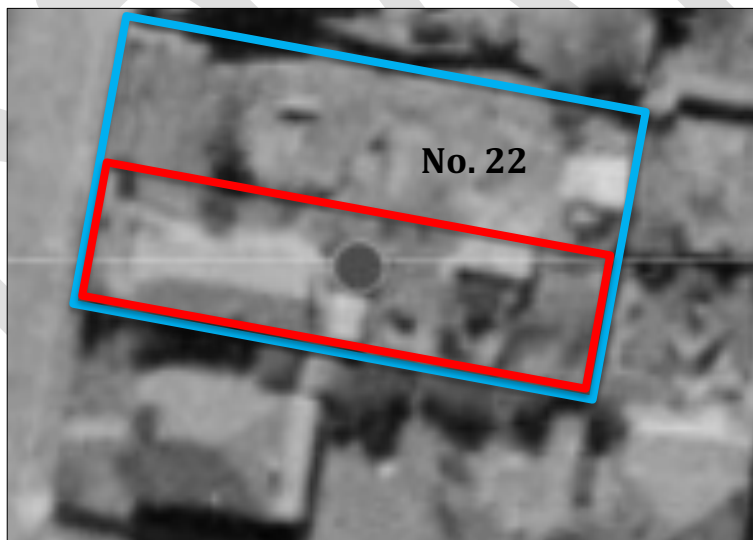
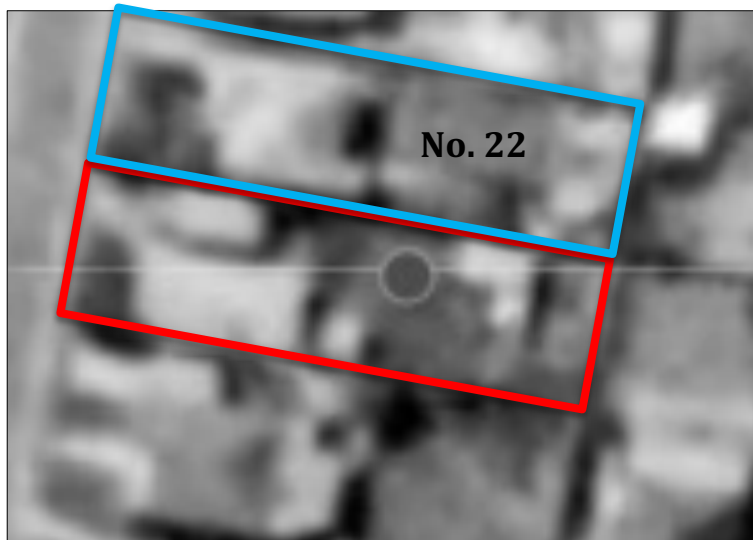


Figure 6: 1955 aerial. The current boundaries of the subject site are outline in red. The apparent boundaries of the site in 1955, comprising No. 22 and No. 24 Railway Street, are outlined in blue. NSW Historic Imagery, portal.spatial.nsw.gov.au, accessed July 2022.



**Figure 7: 1971 aerial. The subject site is outlined in red.
No. 22 Railway Street is outlined in blue.**
NSW Historic Imagery, portal.spatial.nsw.gov.au, accessed July 2022.

3.0 SITE ASSESSMENT

3.1 The Site

For the following, refer to Figure 8, a recent aerial photograph over the site and to the survey that accompanies this application.



Figure 8: Aerial Photograph over the Site, outlined in red. The area outlined in yellow comprises the drainage easement.
SIX Maps accessed August 2022.

The subject site is a rectangular lot. The site has a western (Railway Street) boundary of 12.19m; a rear eastern boundary of 12.19m; a northern side boundary of 40.235m; and a southern side boundary of 40.235m. The site area is 490.5 sq. metres (by survey) and has a rise of approximately 2.8m from the street to the rear eastern boundary. The front boundary is defined by a low concrete black will topped with face bricks. The side boundary fences are marked by *Colourbond* metal panels.

The principal form is set close to the southern side boundary. A free-standing timber framed garage is located between the principal form and the northern side boundary.

The principal form and garage are set close to the front boundary providing for a small front garden and a deeper rear yard.

The northern side of the front garden is dominated by a paved driveway that also provides pedestrian access to the northern side courtyard, and the front entrance porch. To the south of the driveway and pedestrian paths is a wide garden bed with two mature trees and several shrubs.

The north facing courtyard adjacent to the principal form has a concrete finish and planting adjacent to the boundary. There is a small shed and a larger brick structure in the rear yard. The rear yard has numerous mature trees and areas of lawn.

Figure 9 and Figure 10 illustrate the site.



Figure 9: View looking through the front garden.
Realestate.com.au, accessed August 2022.



Figure 10: View of the rear yard.
Realestate.com.au, accessed August 2022.

3.2 The Dwelling

3.2.1 Exterior

The principal dwelling on the subject site presents a modest face brick structure with a gable end to the Railway Street streetscape. The roof is clad in corrugated metal roof sheeting and there is a simple chimney with a terracotta pot on the southern side of the roof. The principal elevation is the western elevation addressing Railway Street. The front door, which has a metal awning over it, is located in this elevation. It is accessed via a small, elevated porch with a metal balustrade. Windows in the front elevation are fitted with security bars. The dwelling has a simple rectangular footprint and aluminium framed windows asymmetrically located around the building.

The two outbuildings in the rear yard have a similar construction. The separate garage on the northern boundary is constructed from timber studs and is lined externally with fibre cement sheets (or similar). The garage has a sheet metal roof and a steel roller door facing Railway Street.

Figure 11 illustrates the exterior of the dwelling.



Figure 11: Front Elevation.
Google Maps accessed August 2022.

3.2.2 Interior

The layout of the dwelling is best understood with reference to Figure 12. The room names used in this plan are adopted in the description below.

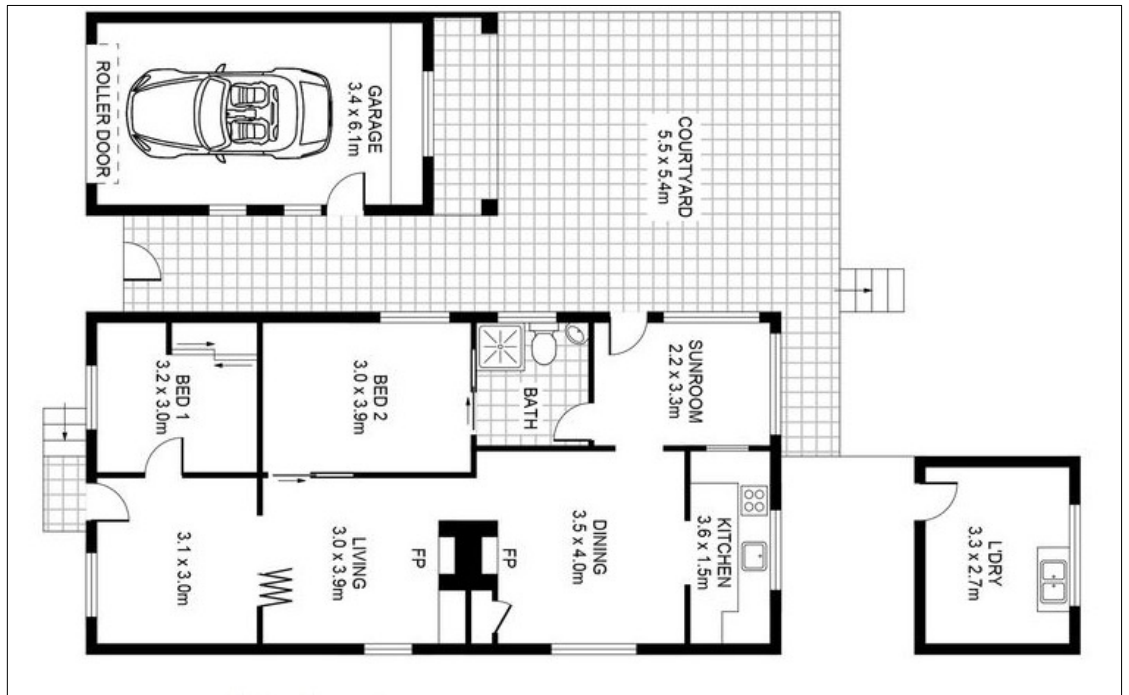


Figure 12: Existing Ground Floor Plan
Domain.com.au, August 2022.

There are 8 rooms in the principal form: two bedrooms, a sunroom, one bathroom, a kitchen, dining room, and a living room and entrance / lobby. The laundry is located in the shed adjacent to the rear elevation. There is a face brick fireplace between the living and dining rooms. The bathroom is contemporary. Finishes throughout the building vary. For example, there are pressed metal ceilings in the dining room, fc sheeting (or similar) with battens in the living room, and gyprock ceilings in the bathroom. Some rooms have square set ceilings while others have a timber moulding.

Figure 13 and Figure 14 illustrates the interior of the dwelling.

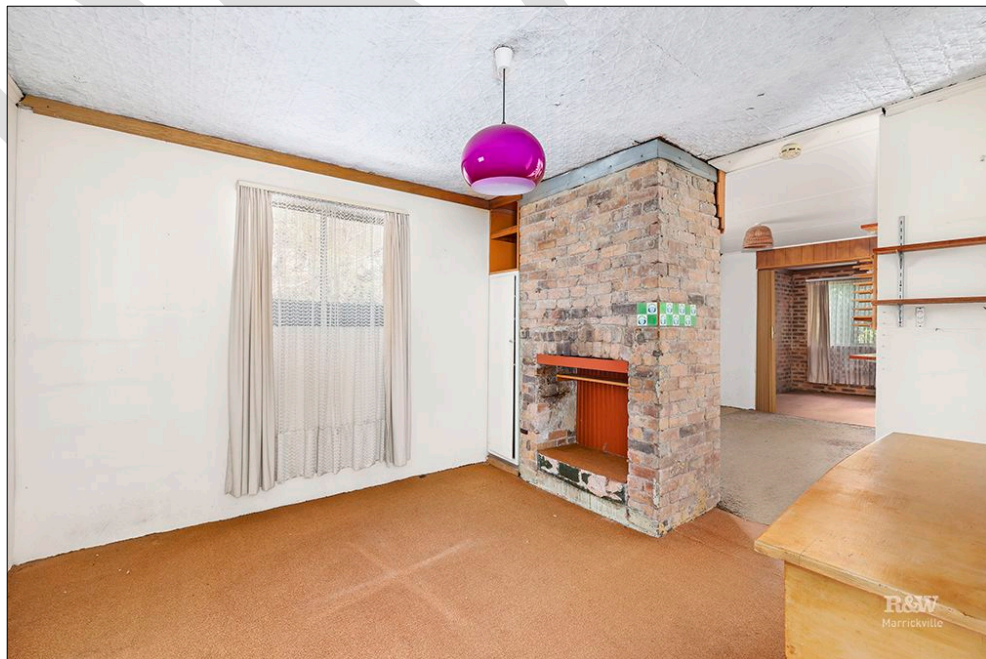


Figure 13: View of the ground floor room overlooking the rear yard.
Domain.com.au, August 2022.



Figure 14: Bathroom
Domain.com.au, August 2022.

The garage walls and ceiling are unlined. The floor is concrete.

Figure 15 illustrates the interior of the garage.



Figure 15: View looking through the freestanding garage toward Railway Street.
Domain.com.au, August 2022.

3.3 The Surrounding Area

For the following, refer to Figure 16, an aerial photograph over the site and the surrounding area. The arrow identifies the site.



Figure 16: Aerial photograph over the site and the surrounding area. No. 24 Railway Street is outlined in red and marked by the red arrow.

SIX Maps accessed August 2022.

3.3.1 The General Area

The site is located within the Railway Street Heritage Conservation Area. The heritage listing sheet for the Conservation Area describes it as follows:

‘The Railway Street Heritage Conservation Area consists of a mixture of single storey Pre-Federation, Federation and Inter War period dwellings, which range from more modest workers cottages to very ornate examples with Australian-themed detailing and sandstone undercrofts. Original details remain in many instances, such as front verandahs, brick boundary walls, original roof forms, face brickwork and detailed gable infill. The area is further characterised by its landscape context which is defined by its mature plantings in proximity to the railway.’²

The *DCP 2012* identifies the following elements as contributing to the Railway Street Heritage Conservation Area streetscape:

- ‘A cohesive group of Inter-war period dwellings at 5A to 15 Foord Avenue, which retain their overall form and scale, original facebrick facades, brick verandah columns, windows, awnings, and decorative gable infills. Some retain their original roofs and finials. A low-set brick boundary wall with textured brickwork runs the length of these properties and further enhances their cohesive appearance.
- Inter-War period workers cottages at 2 to 10 Railway Street, which retain their original face brick facades, gable infill decoration, verandah columns, windows, and overall form.
- Federation period dwellings with intricate timber fretwork and decorative gable infills at 7, 9, and 11 Burnett Street.
- Heritage item ‘Hopetoun Street Group’ (Items I136, I137, I138, and I139) at 19, 23, 25 and 27 Hopetoun Street, which comprises a group of ornate Federation period dwellings with sandstone base, face brick facade, sandstone boundary walls, and chimneys.
- Original details of houses such as:

² *Railway Street Heritage Conservation Area*. State Heritage Inventory Database No.:5067879.

- Front verandahs with original detailing.
- Original roof forms, some with original cladding of slate or unglazed terracotta tiles and original chimneys.
- Gable ends facing the street with original timber shingled, roughcast stucco or imitation half-timbered finishes (Federation and Inter-war periods).
- Face brickwork (Federation and Inter-war periods) and rendered or painted brickwork or weatherboard (Victorian period).
- Original timber-framed windows and timber paneled doors consistent with the periods and styles of houses.³

3.3.2 Railway Street

Railway Street extends in a north south direction from Burnett Street. The street turns to the east where it terminates in a cul-de-sac. The street carries traffic in both directions. Parking on both sides of the street is permissible. There are concrete kerbs and guttering on both sides of the street. There is a concrete footpath on the northern side of the street, there are no street trees on this side of the street. The western side of the street, in the vicinity of the subject site, comprises a deep nature strip with lawn and dense mature vegetation.

The subject site is located on the eastern side of the street close to the intersection with Burnett Street.

Figure 17 and Figure 18 illustrate the general character of Railway Street in the vicinity of the subject site.



Figure 17: View looking north at the Burnett and Railway Street intersection. The subject site is marked by the red arrow.

Google Maps accessed August 2022.

³ DCP 2012, B8 p.195.



Figure 18: View looking south along Railway Street. The subject site is marked by the red arrow.

Google Maps accessed August 2022.

3.3.3 Neighbouring Properties

To the south of the subject site lies No. 26 Railway Street. A single storey face brick Federation style dwelling with a gabled roof clad in terracotta tiles is located on this property. There is a side drive adjacent to the northern boundary and a single car garage that sits in line with the front setback of the principal form. The *DCP 2012* has ranked this property as being a *Contributory 1* item within the Heritage Conservation Area. Figure 19 illustrates this property.

To the north of the subject site lies No. 22 Railway Street. A single storey face brick c1970s style dwelling with a hipped and tiled roof is located on this property. There is a side drive adjacent to the northern boundary and a single car carport that sits in line with the front setback of the principal form. The *DCP 2012* has ranked this property as being a *Non-Contributory* item within the Heritage Conservation Area. Figure 19 illustrates this property.



Figure 19: View looking east across Railway Street toward the subject site, marked by the red arrow.

Google Maps accessed August 2022. Annotations by WP Heritage.

Directly opposite the site, on the western side of Railway Street, lies the rear boundary of No. 13 Foord Street. A single storey face brick Federation style dwelling with a gabled roof clad in concrete tiles is located on this property. There is a single car garage in the rear yard which is accessed via Railway Street. The *DCP 2012* has ranked this property as being a *Contributory 1* item within the Heritage Conservation Area. Figure 20 illustrates this property as seen from Railway Street.



Figure 20: View looking west across Railway Street toward the rear boundary of No. 13 Foord Avenue, marked by the red arrow.

Google Maps accessed August 2022. Annotations by WP Heritage.

The rear boundary of the subject site adjoins the western side boundary of No. 11 Burnett Street. A single storey face brick Federation style dwelling with a timber weatherboard first floor addition, and a hipped and gabled roof clad in Colourbond roof sheeting is located on this property. There is a side drive adjacent to the western boundary and a single car garage that sits in line with the front setback of the principal form. The *DCP 2012* has ranked this property as being a *Contributory 2* item within the Heritage Conservation Area. Figure 21 illustrates this dwelling.



Figure 21: View of No. 11 Burnett Street. The red arrow marks the direction of the principal form within the subject site.

Google Maps accessed August 2022.

4.0 ASSESSMENT OF SIGNIFICANCE

4.1 Summary of Existing Citations and Listings for the Site

No. 24 Railway Street, Hurlstone Park:

- Is not listed as a heritage item by Schedule 5 Part 1 of the *Canterbury LEP 2012*.
- Is located within the Railway Street Heritage Conservation Area as defined by Schedule 5 Part 2 of the *Canterbury LEP 2012*.

The site:

- Is not listed as a heritage item on the State Heritage Register under the auspices of the NSW Heritage Act 1977.

4.2 Heritage Items in the Vicinity of the Site

For the following, 'in the vicinity' has been determined with reference to physical proximity, existing and potential view corridors and the nature of the proposed works.

4.2.1 State Heritage Register

There are no heritage items listed on the State Heritage Register or a s170 Register, under the auspices of the *NSW Heritage Act 1977*, within the immediate vicinity of the site.

4.2.2 Canterbury LEP 2012

Figure 22 shows the location of heritage items, listed by Schedule 5 Part 1 of the *Canterbury LEP 2012*. Heritage items are coloured brown and numbered. Conservation Areas are hatched in red or green and numbered. The subject site is outlined in blue.

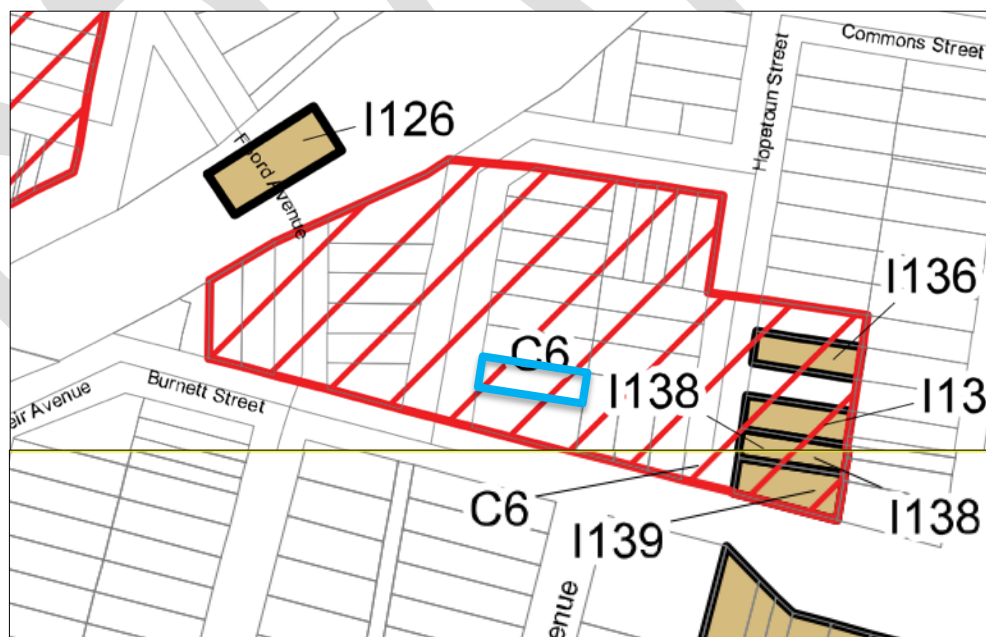


Figure 22: Detail of the Canterbury Bankstown Heritage Map Sheet HER_009 & HER_010.
The subject site is outlined in blue.

Canterbury LEP 2012; annotations by WP Heritage and Planning.

The following heritage items, identified by Schedule 5 Part 1 of the *Canterbury LEP 2012*, lie within the vicinity of the site:

- ***Railway Underbridge, Foord Avenue, Hurlstone Park. Local significance, I126 in Figure 22.***

- ***Federation house, No. 19 Hopetoun Road, Hurlstone Park. Local significance, I136 in Figure 22.***
- ***Federation house, No. 23 Hopetoun Road, Hurlstone Park. Local significance, I137 in Figure 22.***
- ***Federation house, No. 25 Hopetoun Road, Hurlstone Park. Local significance, I136 in Figure 22.***
- ***Federation house, No. 19 Hopetoun Road, Hurlstone Park. Local significance, I138 in Figure 22.***
- ***Federation house, No. 27 Hopetoun Road, Hurlstone Park. Local significance, I139 in Figure 22.***

These items are considered to be sufficiently removed from the site by distance and intervening buildings for works of the proposed nature to have no impact on significant view corridors to/from it or on the character of its setting and hence on its significance. It is not considered further for the purposes of this statement.

4.3 Identification of View Corridors

The principal view corridors towards the subject site are obtained from directly in front of it on Railway Street; notwithstanding the mature trees planted in the front garden of the subject site.

On approach from the south, the front boundary fence, front elevation, part of the southern elevation and the principal roof form can be seen from Burnett Street.

On approach from the north, the principal form is completely concealed by the mature trees planted in the front garden of No. 22 Railway Street and within the front garden of the subject site.

There are no views toward the rear of the property or the rear roof plain of the principal form.

View corridors out of the site are to the west and Railway Street and toward Burnett Street.

4.4 Integrity & Condition

The existing dwelling on the site appears to have been constructed in c.1908, however, it has been substantially and irretrievably modified including a completely replaced roof profile and front elevation to Railway Street so that it now reads as c.1970s style dwelling. The freestanding laundry and rear outhouse were constructed post 1943, and the garage was constructed c.1970.

As per Section 4.5.2 below, the subject site is not a contributory item within the Railway Street Heritage Conservation Area.

Whilst the existing dwelling appears to be in fair condition, no further discussion is warranted as demolition of Non-Contributory items is permissible without justification.

4.5 Statement of Significance

4.5.1 For the Railway Street Heritage Conservation Area

The State Heritage Inventory provides the following Statement of Significance:

‘The Railway Street Heritage Conservation Area is of local heritage significance.

The area is of historical significance as an area developed between the late 19th and early 20th century and which includes:

- Section 17 of Sophia Campbell's original 1876 subdivision of Hurlstone Park.
- William Augustus Starkey's 1878-1879 purchase of four allotments (Lots 4 and 8) of Section 17.
- The western half of the 1901 1st Subdivision of Jeffreys Estate (DP 3849).

The area is also of historical significance as having been the location of stone quarries in the mid-20th century, which is reflected in both the steep landscape context and incorporation of sandstone into a number of dwellings along Hopetoun Street.

The area is of aesthetic significance as its close proximity to the railway has resulted in a transitional architectural character. This ranges from the more modest Federation and Inter-war period workman's cottages located within the vicinity of the railway including 5A to 15 Foord Avenue and 2 to 10 Railway Street; to the increasingly ornate Federation-period dwellings found at higher elevations such as 7 to 11 Burnett Street, and 19, 23, 25 and 27 Hopetoun Street.

It is also of aesthetic significance for its historical plantings and stepped landscape character, with wide, shaded streets which descend toward the railway line. The steep landscape context of the area facilitates elevated views over the station from Hopetoun Street.⁴

4.5.2 Contribution to the Conservation Area

Section 8.5: Hurlstone Park Heritage Conservation Area of the Canterbury DCP 2012 provides the following definitions for Contributory and Non-Contributory buildings:

'Contributory 1

A building that clearly reflects the heritage values of a key period of significance for the HCA (for example a well-conserved Federation Queen Anne style house).

Contributory 2

A building that has been altered but is still identifiable as dating from a key period of significance for the HCA and retains its overall form (for example a Federation Queen Anne style house that has altered window openings, cement rendered walls).

Non-Contributory

Either a building that does not date from a key period of significance for the area (for example a late 20th century or more recent building) or a building that has been so altered the period of construction is no longer readily discernible.⁵

No. 24 Railway Street was constructed in c. 1908, however, the building has been significantly and irretrievably altered such that the building now reads as a c.1970s style structure. The subject site is therefore considered a Non-Contributory item within the Conservation Area. Further, No. 24 Railway Street has also been assessed and ranked as a Non-Contributory item by the *Area Character Statement: Railway Street Heritage Conservation Area* in the *DCP 2012*⁶.

Section 8.5.18 of the *Canterbury DCP 2012* states that:

⁴ *Railway Street Heritage Conservation Area*. State Heritage Inventory Database No.:5067879.

⁵ *Canterbury DCP 2012*, Heritage B8, p.129.

⁶ *Canterbury DCP 2012*, Heritage B8, p.197.

‘Demolition of non-contributory buildings within heritage conservation areas is permissible.’⁷

5.0 HERITAGE IMPACT STATEMENT

5.1 Scope of Works

The following should be read in conjunction with the plans prepared by Fairmont Homes that accompany this application.

The following works are proposed:

- Demolition of all structures on the site.
- Construction of a new single storey dwelling with a carport.
- Construction of new street boundary fencing.
- Associated landscape works.

The proposed external finishes include:

- Walls of face brick.
- Walls of painted weatherboard.
- Corrugated metal roof sheeting.
- Aluminium framed windows.

5.2 Method of Assessment

The following considers heritage related issues only. It does not consider compliance or otherwise with Council’s numerical controls unless non-compliance will result in an adverse heritage impact. For such an assessment, refer to the Statement of Environmental Effects (SEE) that accompanies this application.

The proposal is assessed against the objectives and controls of Section B8 Heritage in Part B General Controls of the *Canterbury Development Control Plan 2012 (Canterbury DCP 2012)*.

5.3 Effect of Work on the Railway Street Heritage Conservation Area

The proposed works will have no impact on the ability to understand the historical significance of the Railway Street Heritage Conservation Area for the following reasons:

- There will be no impact on the subdivision pattern or on the topography.
- The removal of the existing building will have no impact on the ability to understand the historic and aesthetic significance of the Conservation Area. While the extant building dates from c.1908, it has been extensively and irretrievably altered so that the period of construction is no longer readily discernible. Council has previously assessed the site as being a Non-Contributory item within the Conservation Area.
- The historic use of properties fronting Railway Street, for residential purposes, will not be changed by the proposed works.
- The proposed works are considered sympathetic to the Railway Street Heritage Conservation Area because the proposed replacement dwelling will maintain the rhythm and scale of the streetscape. The proposal maintains the pattern of consistent front setbacks, single storey front elevations, front verandahs, pitched roofs with gable ends facing the street, and mixed material palettes; these are the principal elements that determine how the site relates to the adjoining properties and how it reads from within the public domain.

⁷ *Canterbury DCP 2012*, Heritage B8, p.146.

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- The proposed dwelling will not have undue prominence in the Conservation Area. The dwelling is well articulated, utilizes a sympathetic material and colour palette, and will authentically reflect the contemporary times in which the works are constructed.
 - The proposed replacement street boundary fencing, whilst clearly contemporary, is more sympathetic than the existing, it will be high quality, and it will be consistent with the proposed replacement dwelling. The proposed front boundary fence utilises a mixed material pallet, is a similar height to fences on neighbouring properties, is partially open style, and facilitates views towards the principal elevation addressing Railway Street.
 - The proposed building finishes are not such that the building will have undue prominence in the wider setting of the Conservation Area.
 - There will be no impact on view corridors into/out of/or within the Conservation Area, in particular the significant views along Railway Street will not be impacted upon.

The proposed works will thus have no impact on the significance of the Railway Street Heritage Conservation Area.

6.0 CONCLUSION

This HIS has considered the impact of the proposed demolition of the existing dwelling on the subject site, and the construction of a new dwelling at No. 24 Railway Street, Hurlstone Park, New South Wales.

Constructed in c.1908, No. 24 Railway Street, Hurlstone Park presents a much-altered principal form to the streetscape; the period of construction is no longer readily discernible and the changes to the place have no architectural merit. Council has previously assessed the site as being a Non-Contributory item within the Conservation Area, thus, demolition is permissible.

The proposed new dwelling is contemporary but sympathetic in form and detailing. It will present a well-mannered and high-quality structure to the streetscape. It will not adversely affect or unacceptably compromise, detract from, obscure, or distort the cultural significance of the Railway Street Heritage Conservation Area.

The proposal fulfils the objectives for works to properties in Conservation Areas as set out by the *Canterbury LEP 2012* and the *Canterbury DCP 2012*.